

City of Topeka
Brick Street, Alley and Sidewalk Policy

Purpose: This policy is to provide guidance for the preservation and maintenance of existing exposed brick streets, alleys, and sidewalks.

Application: This policy requires that anyone that disturbs or excavates the surface of an existing exposed brick street, alley or sidewalk will be responsible for the replacement/restoration of the surface in the same installation pattern and with brick of a like kind and color. The streets with exposed brick surfaces designated on the attached map will be subject to the preservation, repair and reconstruction provisions of this policy and will be referred to herein as "Brick Streets." Other streets and alleys which have a brick base but have been overlaid with asphalt will be subject to the brick salvage provisions. This policy does not apply to City improvement projects where brick elements may be replaced with a different material after appropriate approvals. Any excavation within the Right of way (ROW) must be repaired in accordance with (IAW) Topeka Municipal Code 12.30.120 Right-of-way repair and restoration.

Effective Date: 7 / 18 / 2011

Office of Primary Responsibility: Public Works – Director and City Engineer or his/her representative – oversight of design specifications, materials, and construction inspection.

Policy: There will always be a need for ongoing repair, installation and maintenance of utility lines which requires cutting into brick streets, alleys and sidewalks. Nearly all exposed brick streets are within neighborhoods that have adopted planning policies to preserve brick streets. Furthermore, it is the desire of the city to maintain the character of our existing neighborhoods by preserving the exposed brick streets, alleys and sidewalks as an historic asset as much as possible. (NOTE) If at any time, anyone observes that a utility company, private contractor or any other party excavating the right-of-way is not completing work in accordance with this policy, or if bricks are not being handled properly or are being stolen or vandalized, a report may be made to the Office of Public Works Administration at 785-368-3801.

I. Utility Cuts, Repair, Reconstruction, and Resurfacing

A. Brick Streets Designated for Preservation

In an effort to preserve the character of existing brick streets, any time an exposed brick street requires maintenance, including either surface repairs or underlying utilities, all patching shall be completed using brick of like kind and color in accordance with this policy unless meeting one of the exceptions listed below:

1. The Public Works Director shall have the discretion to replace bricks with an alternative non-brick material in blocks that are not impacted by designated state or national historic properties and their environs; that are inconsistent with a neighborhood plan; or that have excessive non-brick patching and are rated in "poor" condition.
2. The Public Works Director shall have the discretion to temporarily deviate from these criteria in order to protect public safety or in emergencies.
3. Prior to any deviation of this policy within the vicinity of a designated state or national historic property and its environs, or in an historic district, the Director of Public Works Department shall first consult with the Topeka Landmarks Commission or State Historic Preservation Office as applicable.
4. Any deviation from a street's designation included in an adopted Neighborhood Plan Element of the City's Comprehensive Plan will first require action by the Planning Commission and City Council in accordance with State Law.

B. Brick Streets Not Designated for Preservation

Streets with exposed brick surfaces which are not designated for preservation are subject to Section III Salvaging Bricks of this policy. Following completion of utility work or construction, the street may be repaired using asphaltic concrete or other material approved by the Public Works Director.

C. Asphalt Overlay on Brick Streets

Streets with asphalt overlays which have underlying bricks are not designated for preservation and are therefore subject to Section III Salvaging Bricks of this policy. Following completion of utility work or construction, the street may be repaired using asphaltic concrete or other material approved by the Public Works Director.

D. Alleys

All brick alleys are subject to Section III Salvaging Bricks of this policy and following completion of utility work or construction, the alley may be repaved with asphaltic concrete. However, if the exposed brick alley is a contributing factor to a registered state or national historic property and its surrounding environs, or an Historic District, the alley shall be preserved as much as possible in accordance with the Brick Patching Standards provided as Attachment 1.

E. Sidewalks

When reconstruction or repair of brick sidewalks located in the public right-of-way is required in a localized area or spot repair of 15 feet in length or less, the sidewalk shall be reconstructed using brick of like type and color and laid in the same pattern as the adjacent sidewalk. In larger areas, the sidewalk may be reconstructed or replaced with concrete unless the particular section of brick sidewalk meets one of the following conditions:

1. Is located on a block within the vicinity of a designated state or national historic property and its environs and has not received approval of the Topeka Landmarks Commission for concrete replacement or similarly approved alternative;
2. Is located within or adjacent to an Historic District or designated Local Landmark;
3. Is located on a block within a Neighborhood Plan Element of the City's Comprehensive Plan and is recommended to be preserved as a brick sidewalk consistent with said plan;
4. Anywhere not addressed in #1, #2, #3 but located on a block where the sidewalk on one side of the street is at least 60% or more brick sidewalk and is maintained to level and safe conditions;
5. The property owner adjacent to the public sidewalk does not agree to replace the brick sidewalk with a concrete sidewalk and said property owner has demonstrated a history of adequately maintaining the existing brick sidewalk to level and safe conditions.

F. General

1. If one of the conditions listed under E exists, the sidewalk shall be replaced or reconstructed in brick. However, if the property owner adjacent to the City-owned sidewalk does not agree to replace the brick sidewalk with a concrete sidewalk (Condition #5 above) and none of the other conditions are applicable, the owner shall be required to pay the additional cost to reconstruct the sidewalk in brick versus concrete.
2. The Planning Department shall review any City-initiated projects for consistency with criteria #1, #2, and #3 of E and be notified by the Public Works Department prior to replacing any City-owned brick sidewalks with concrete. No project shall proceed inconsistent with State or local law. Any deviation from condition #3 shall first require the consultation of the Public Works Director and the Planning Department Director (or their representatives) with the effected residents, property owners, and neighborhood association.
3. Notwithstanding the restrictions set forth in this section concerning the retention of brick sidewalks, those sections of brick sidewalk need not be retained where the Public Works Director finds that due to changes in the vicinity of a specific request for waiver, brick sidewalk no longer serves as an enhancement. In such cases, waiver may be granted by the Public Works Director.
4. If the brick sidewalk passes through a driveway or alley, standard driveway or alley construction shall be used, in accordance with the City Standard Technical Specification Manual.

G. Other Accessories

1. **Concrete Curb and Gutter** – Curb repair and replacement must comply with the City of Topeka, Engineering Division Standard Technical Specification Manual for curb and gutter. Re-laid bricks shall abut the new concrete gutter and be installed in accordance with the City Standard Technical Specification Manual. Details are provided as Attachment 2.
2. **Stone Curbs and Brick Gutters** – Stone curbs and brick gutters shall be preserved “in place” as much as possible. On brick streets where existing or newly installed stone curbs are constructed, brick pavers will be installed in areas where the brick gutter abuts the stone curbs in accordance with the City Standard Technical Specification Manual (Attachment 2).
 - a. **Preservation of Existing Stone Curbs** – In areas of reconstruction involving existing streets where curbs are constructed of stone, the stone curb structures shall be retained if Conditions #1, 2, and 3 from the Sidewalk section above are met. Sections of qualifying stone curb in need of repair shall be replaced using stone of like kind if available, otherwise, precast concrete block made to match the height, color and character of adjacent stone curbs shall be used (detail in Attachment 2). In non-qualifying areas, stones may be removed and salvaged where new curb and gutter, driveways, or wheelchair ramps are to be constructed.
 - b. **Removing & Salvaging Stone Curbs:**
 - Prior to disturbing any stone curb, the City Street Maintenance Section shall be notified. The curb stones shall be removed, salvaged and inventoried. The curbs shall be stored at a location designated by the Street Maintenance Section.
 - No metal tools, equipment, or implements shall be used to pry, loosen, move or lift the curb stones unless the attachments are protected to prevent damaging the stone.
 - All removable marks of any kind imposed on the curb stone exposed face or top by the contractor shall be removed by soft cloth, nylon brush, and water.
 - All earth shall be hand-water-washed from the curb stone.
 - All mortar shall be hand removed by wooden or hard, rubberized tools.
3. **Crosswalks** – Where a crosswalk provides access by sidewalks, curb ramps that meet ADA Accessibility Guidelines (ADAAG) must be provided at both ends of the crosswalk. The crosswalk width should be 10 feet wide and shall not be less than 6 feet.
 - a. On brick street surfaces, the installation of colored and textured pavement may be used to enhance the aesthetics of the crosswalks. All alternative surface construction applications will be considered.
 - b. Any alternative crosswalk material must be approved by the Director of Public Works and application must be completed by appropriately trained city personnel or certified contractor. Installers must follow all the manufactures specifications and application methods.

II. Utility Requirements

- A. Utility cuts are the most common surface disturbance affecting brick streets and alleys. Cuts for utility maintenance can be made by the following but is not limited to: public utilities, franchise utility companies, and private service lines. Patches on exposed brick streets, alleys or sidewalks will be restored with brick immediately after the repairs are completed, to be done by, and at the expense of the party responsible for the work. Construction Inspection will inspect the utility cuts on all brick surfaces for quality and timeliness of the repairs. The following are examples of separate entities that may disturb the surface of those pavement penetrations:
 1. **Utility Companies** – Excavations within the public right-of-way made by franchise utility companies require a permit from the Public Works Department. Patches that are made as a result of utility cuts shall be restored *IAW Sec. 6, Use of Right-of-way* as noted in each utility’s franchise agreement. The utility will pay the entire cost of

restoration and bricks shall be placed back in accordance with the Brick Patching Standards (Attachment 1).

2. **Private Contractor** – Excavations within the public right-of-way made by private contractors require a permit from the Public Works Department, and are normally done as a paid service for residents. Any excavation within the (ROW) must be repaired *LAW Topeka Municipal Code 12.30.120 Right-of-way repair and restoration.*
3. **City Employee Maintenance** – Excavations made by the City Of Topeka during the course of maintaining the public utilities shall be placed back in accordance with the Brick Patching Standards (Attachment 1) using the appropriate City funds or contracted service.
4. **Failure to Restore** – *LAW Topeka Municipal Code 12.30.120 Right-of-way repair and restoration Item (d)* - If the responsible party fails to restore the right-of-way in the manner and to the condition required by the Director of Public Works, or fails to satisfactorily and timely complete all restoration required by the director, the city, at its option, may do such work. In that event the responsible party shall pay to the city, within 30 days of billing, the cost of restoring the right-of-way. (Ord. 17587 § 5-8, 10-24-00. Topeka Municipal Code 12.30.120.)

III. Salvaging Bricks

- A. Any disturbance to the surface of a brick street, alley or sidewalk shall be patched reusing existing brick from that location to maintain like kind and color. Caution shall be used to prevent damaging bricks. If bricks are not needed for reuse in the repair of the disturbed area, all Public Works employees, utility companies, and other contractors or workers shall be directed to salvage all brick removed from any street, alley or sidewalk.
- B. It is the responsibility of the utility company, contractor or other party excavating the right-of-way to ensure that bricks are safely secured to prevent theft or damage until repairs are completed and bricks are returned and reset.
- C. It is the responsibility of the utility company, contractor or other party excavating the right-of-way to ensure that unused bricks are salvaged in accordance with A above. The party responsible shall palletize bricks and convey to the City for inventory. Bricks shall be delivered to the location designated by the Street Maintenance Section.
- D. All excavation within the city right-of-way will include the requirement for the salvaging of bricks.

IV. Brick Street Inventory Map

- A. Provided as a part of this policy is a Brick Street Inventory Map which illustrates known existing brick streets and asphalt overlaid brick streets. Additionally, the map identifies which existing brick streets are designated for preservation. The map is included as Attachment 3.
- B. The information contained as a part of the Brick Street Inventory Map may not be all inclusive and upon discovery of bricks in streets that are not identified as existing brick streets on the map, the responsible party should report to the Office of Public Works Administration at 785-368-3801.

3 Attachments

APPROVED BY:



Randy L. Speaker
Director of Public Works

Attachment 1

Brick Patching Standards

This standard pertains to exposed brick streets or alleys that will be repaired to their original brick surface appearance.

Prior to removal of any of the brick street or alley surface a representative of the Public Works Department Construction Inspection or Utility Management Section, will mark the limits for the brick street replacement and make the final determination whether the bricks are salvageable. During removal of the existing brick street surface, due care shall be exercised to prevent damage to adjacent bricks.

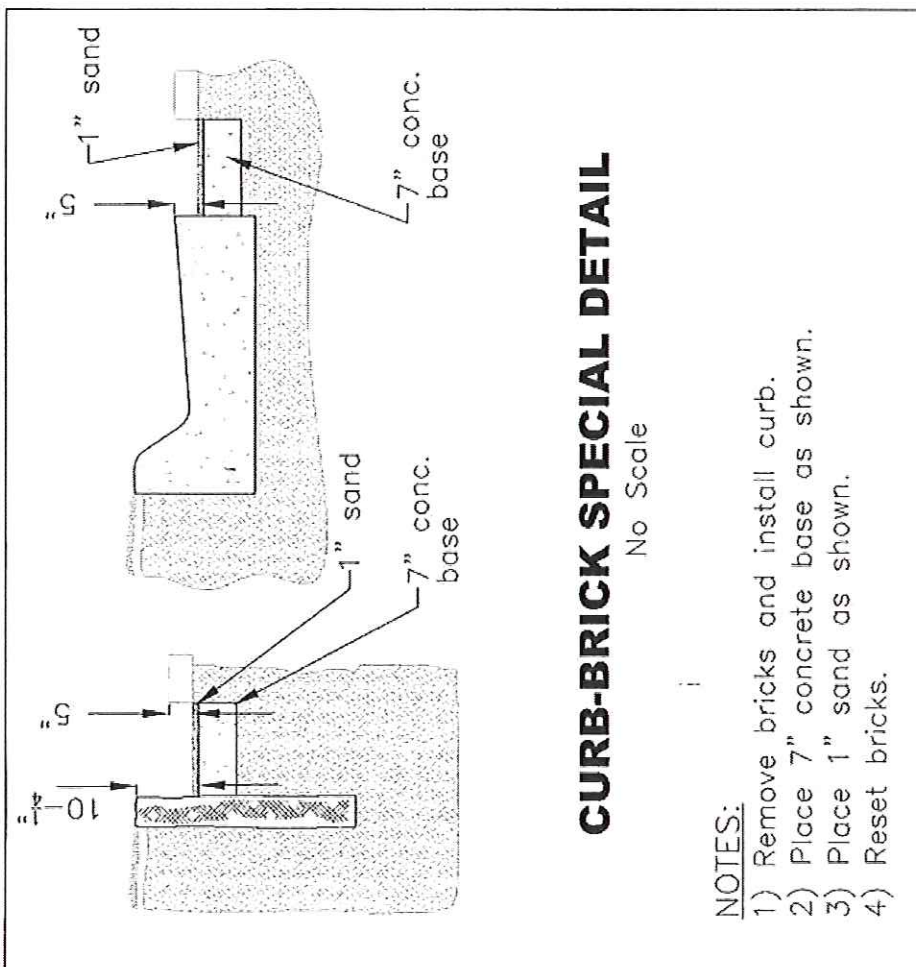
Upon excavation to the depth required for placement of the concrete base course, the existing subbase shall be re-compacted. If the subbase is still unstable as determined by a representative of the Public Works Department, Construction Inspection, or Utility Management Section, it shall be over excavated to a depth of 6" and Aggregate Base – Type AB-3 or flowable fill shall be placed and compacted below the concrete base course.

All repair areas will require placement of a 7" reinforced concrete base course. Other materials or methods may be used at the responsible party's expense with City approval.

An uncompacted leveling base of FA-A, non-plastic, clean sand shall be screeded over the concrete base course to a thickness of 1" to 1½". The bricks are expected to settle ¼" to ½" after compaction.

- Bricks shall be laid to follow the adjacent brick pattern with generally the same spacing between bricks as the adjacent bricks.
- As the bricks are laid they shall be moved back and forth to solidly bed them into the sand leveling base.
- When necessary to cut bricks, cutting shall be performed to leave a clean edge to the traffic surface. Bricks shall be cut with either a block splitter or a masonry saw.
- Once the bricks are in place, sand shall be placed over the area and worked into the joints between the bricks with a broom, leaving a thin sand layer 1/8" to 1/4" thick over the patch area.
- A pass shall be made with a vibratory plate compactor over the brick surface. The compactor shall be a plate type soil compactor capable of 3,500 to 5,000 lb centrifugal compaction force.
- Additional passes shall be made over the area with the vibratory plate compactor while simultaneously brushing additional FA-A sand into the joints until the sand is even with the top of the bricks.
- The patch shall then be watered while adding additional FA-A sand to the area and brooming the sand into the joints. A thin layer (1/4" maximum) of sand shall be left over the patch. All other excess sand shall be removed from the site.
- After 30 days, or as directed by Construction Inspection or Utility Management Section, sand is broomed and watered into the joints, the repair crew shall again broom and water FA-A sand into the brick joints. Excess sand shall be removed from the site.

Brick restoration or patching required by a City project will be competitively bid at price per square yard; the price shall include furnishing all work required to complete the excavation, subgrade improvement if needed, 7" reinforced concrete base course, and reconstruction of the brick pavement.



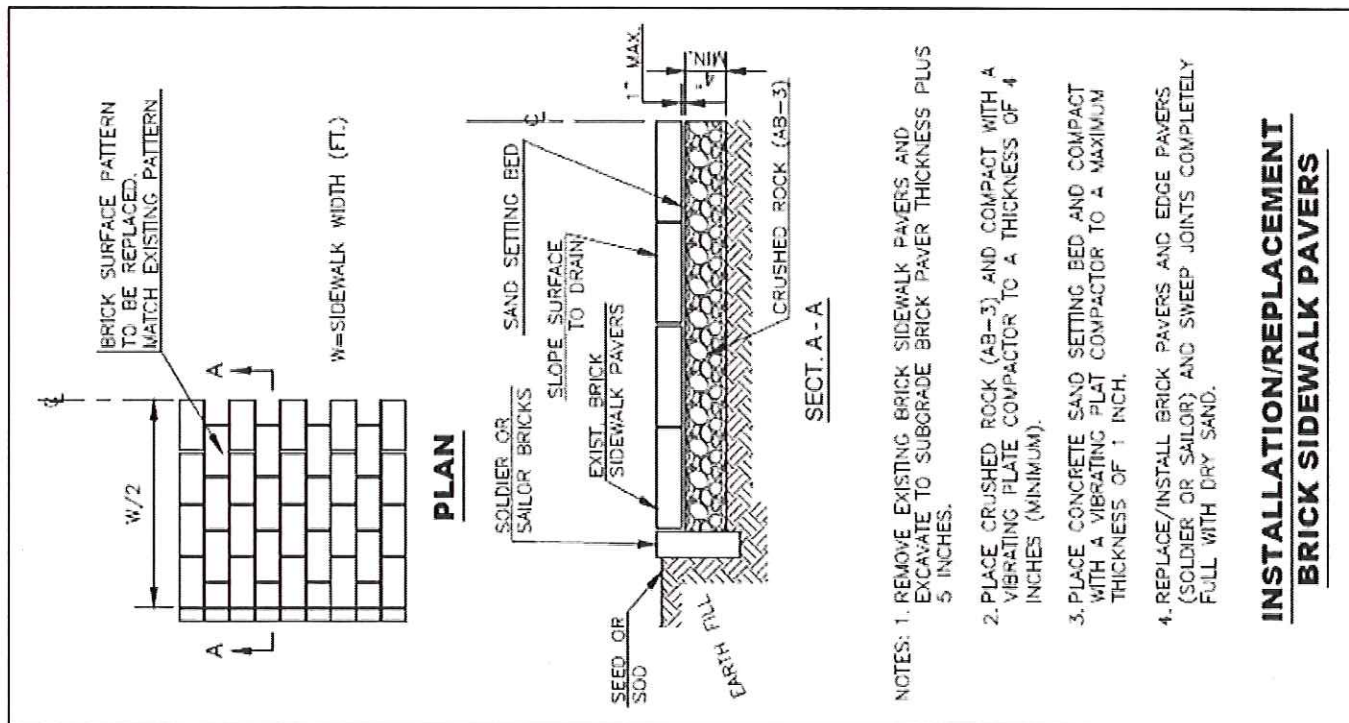
CURB-BRICK SPECIAL DETAIL

No Scale

NOTES:

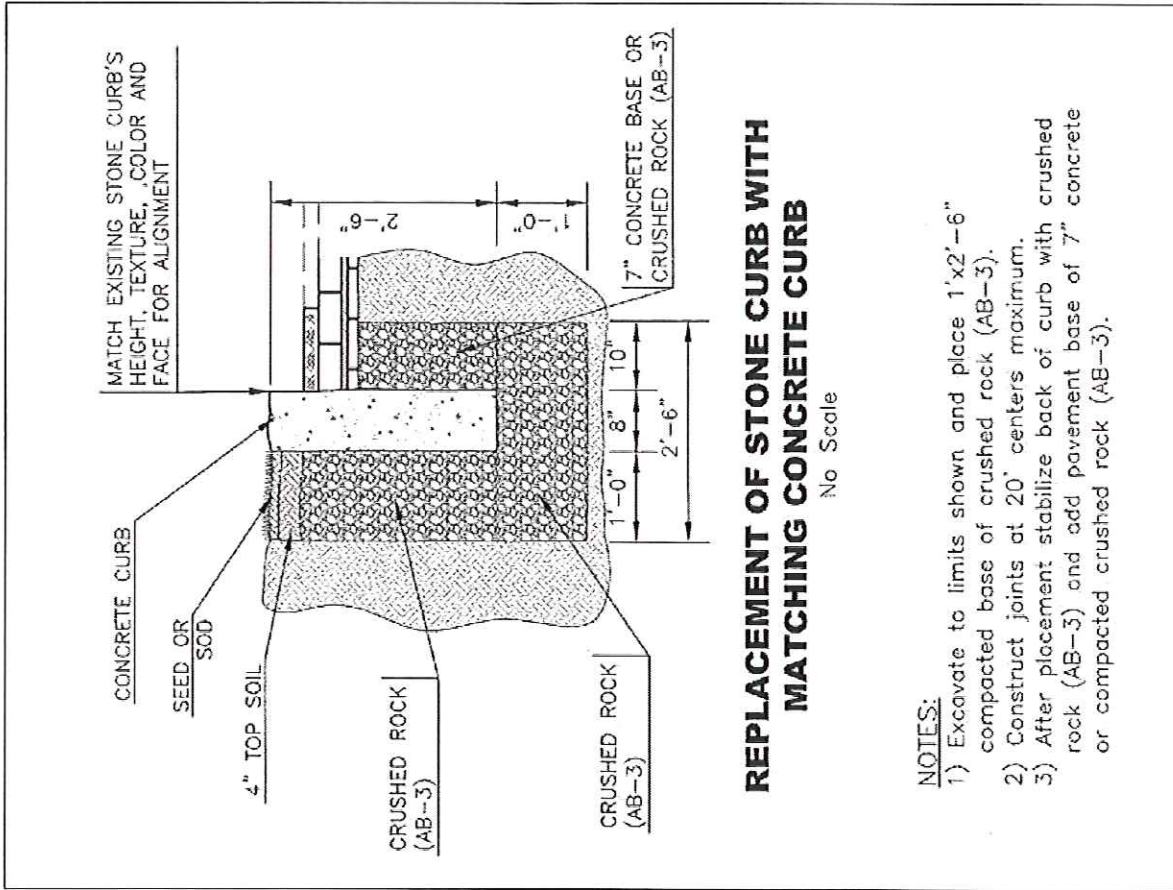
- 1) Remove bricks and install curb.
- 2) Place 7" concrete base as shown.
- 3) Place 1" sand as shown.
- 4) Reset bricks.

(NOTE) At the time of approval of this policy, this detail may not yet have been included in the City Standard Technical Specification Manual.



- NOTES:**
1. REMOVE EXISTING BRICK SIDEWALK PAVERS AND EXCAVATE TO SUBGRADE BRICK PAVER THICKNESS PLUS 5 INCHES.
 2. PLACE CRUSHED ROCK (AB-3) AND COMPACT WITH A VIBRATING PLATE COMPACTOR TO A THICKNESS OF 4 INCHES (MINIMUM).
 3. PLACE CONCRETE SAND SETTING BED AND COMPACT WITH A VIBRATING PLATE COMPACTOR TO A MAXIMUM THICKNESS OF 1 INCH.
 4. REPLACE/INSTALL BRICK PAVERS AND EDGE PAVERS (SOLDIER OR SAILOR) AND SWEEP JOINTS COMPLETELY FULL WITH DRY SAND.

**INSTALLATION/REPLACEMENT
BRICK SIDEWALK PAVERS**



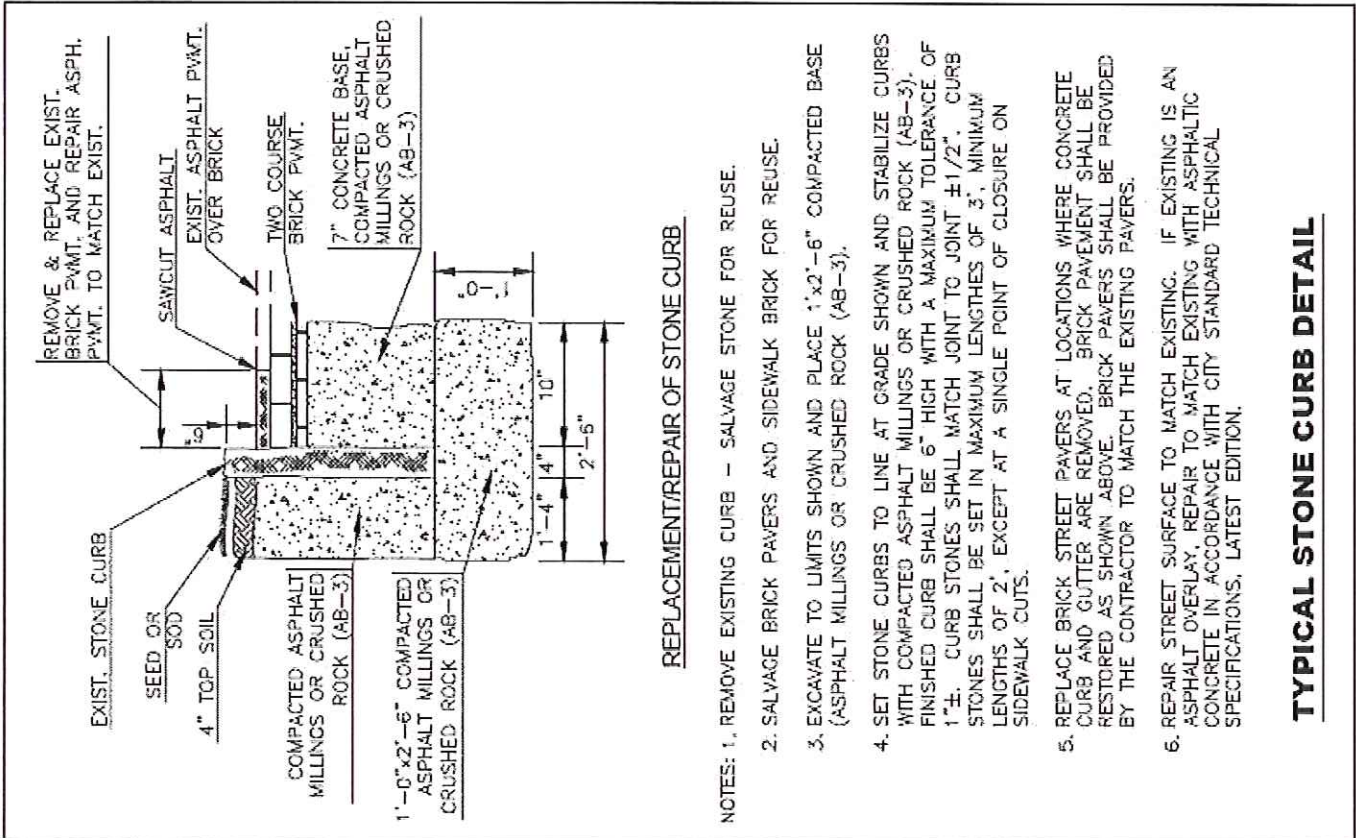
REPLACEMENT OF STONE CURB WITH MATCHING CONCRETE CURB

No Scale

NOTES:

- 1) Excavate to limits shown and place 1'x2'-6" compacted base of crushed rock (AB-3).
- 2) Construct joints at 20' centers maximum.
- 3) After placement stabilize back of curb with crushed rock (AB-3) and add pavement base of 7" concrete or compacted crushed rock (AB-3).

(NOTE) At the time of approval of this policy, this detail may not yet have been included in the City Standard Technical Specification Manual.



REPLACEMENT/REPAIR OF STONE CURB

NOTES: 1. REMOVE EXISTING CURB - SALVAGE STONE FOR REUSE.

2. SALVAGE BRICK PAVERS AND SIDEWALK BRICK FOR REUSE.

3. EXCAVATE TO LIMITS SHOWN AND PLACE 1'x2'-6" COMPACTED BASE (ASPHALT MILLINGS OR CRUSHED ROCK (AB-3)).

4. SET STONE CURBS TO LINE AT GRADE SHOWN AND STABILIZE CURBS WITH COMPACTED ASPHALT MILLINGS OR CRUSHED ROCK (AB-3). FINISHED CURB SHALL BE 6" HIGH WITH A MAXIMUM TOLERANCE OF 1". CURB STONES SHALL MATCH JOINT TO JOINT ±1/2". CURB STONES SHALL BE SET IN MAXIMUM LENGTHS OF 3'. MINIMUM LENGTHS OF 2', EXCEPT AT A SINGLE POINT OF CLOSURE ON SIDEWALK CUTS.

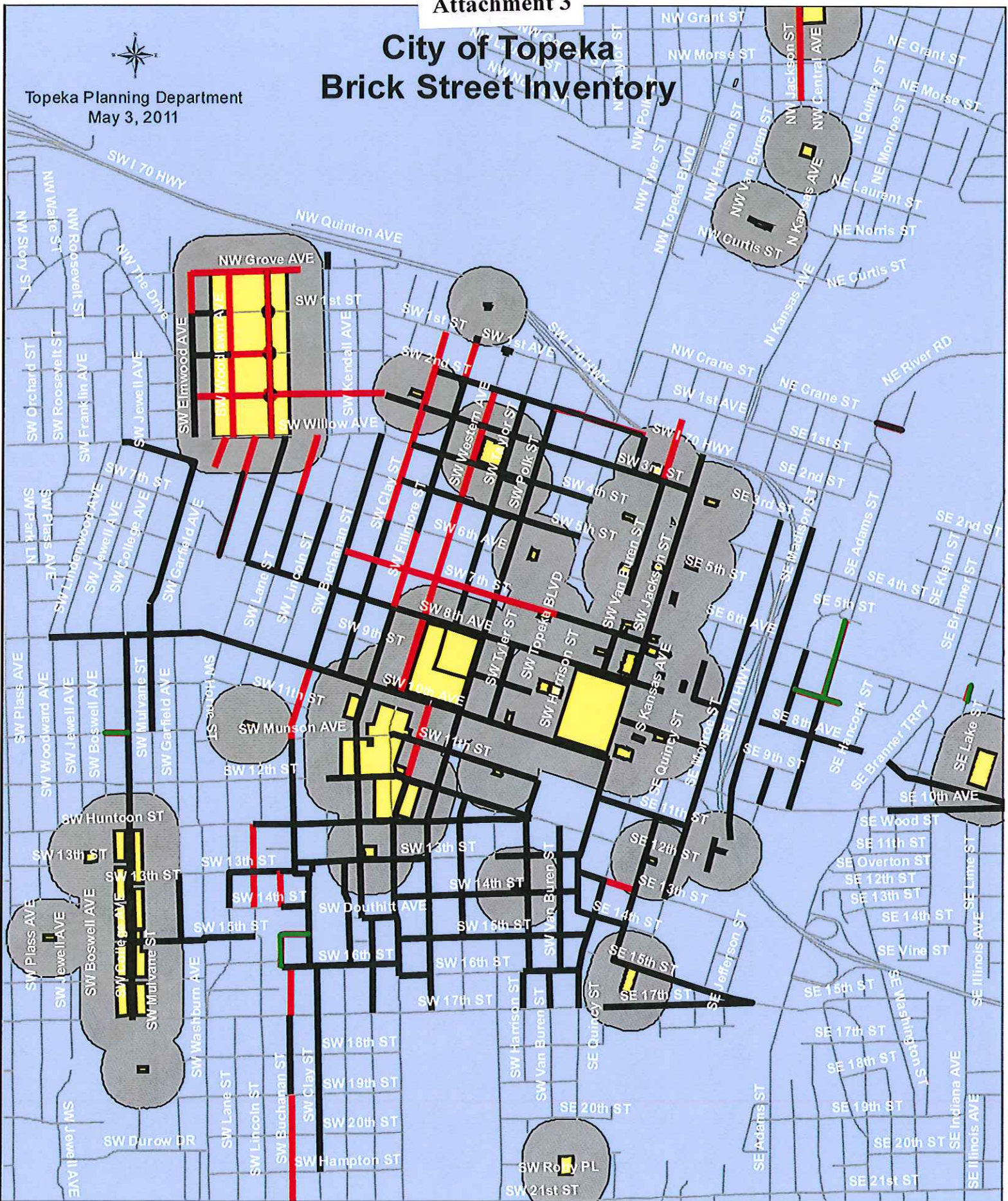
5. REPLACE BRICK STREET PAVERS AT LOCATIONS WHERE CONCRETE CURB AND GUTTER ARE REMOVED. BRICK PAVEMENT SHALL BE RESTORED AS SHOWN ABOVE. BRICK PAVERS SHALL BE PROVIDED BY THE CONTRACTOR TO MATCH THE EXISTING PAVERS.

6. REPAIR STREET SURFACE TO MATCH EXISTING. IF EXISTING IS AN ASPHALT OVERLAY, REPAIR TO MATCH EXISTING WITH ASPHALTIC CONCRETE IN ACCORDANCE WITH CITY STANDARD TECHNICAL SPECIFICATIONS, LATEST EDITION.

TYPICAL STONE CURB DETAIL

City of Topeka Brick Street Inventory

Topeka Planning Department
May 3, 2011



- Brick Streets designated for preservation
- Brick Streets not designated for preservation*
- Asphalt Overlay on Brick Street
- Historic Register Properties
- 500' Historic Environs

*meets Exception #1 criteria under City of Topeka's Brick Street, Alley, and Sidewalk Policy