

Transportation Alternatives (TA) Program
Federal Fiscal Year 2022-23 TA PROJECT CONCEPT

APPLICANT INFORMATION			
1. ELIGIBLE APPLICANT – select only one <input checked="" type="checkbox"/> Municipality <input type="checkbox"/> County <input type="checkbox"/> State Agency <input type="checkbox"/> Federal Agency <input type="checkbox"/> USD <input type="checkbox"/> School <input type="checkbox"/> Tribal Gov. <input type="checkbox"/> Other _____			
2. AGENCY NAME City of Topeka		3. CO-SPONSOR (if any)	
4. AGENCY MAILING ADDRESS 215 SE 7th St.		CITY Topeka	ZIP 66603
5. PRIMARY CONTACT Kristi Ericksen	TITLE/ORGANIZATION City of Topeka Traffic Engineer	EMAIL kericksen@topeka.org	PHONE 785-368-3029
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PROJECT DESCRIPTION		
6. PROJECT NAME Bikeways Phase 4 - Equitable connections		
7. LOCATION/ADDRESS Various locations around Topeka -see attached map		8. PROJECT LIMITS (mileposts, intersecting roadways, rivers, railroads, other boundaries) Various locations around Topeka -see attached map
9. COUNTY Shawnee	10. MUNICIPALITY Topeka	11. PROJECT LENGTH/SIZE (if applicable)
12. 3-4 SENTENCE DESCRIPTION OF PROJECT CONCEPT See following page for answer:		

3-4 SENTENCE DESCRIPTION OF PROJECT CONCEPT

This project will be the fourth phase of our Bikeways Master Plan. Our concept is to select projects that will fill in the gaps, create safer facilities, and provide greater access/equity. The City of Topeka and MTPO (Metropolitan Topeka Planning Organization) has recently completed a supplement to the Bikeways Master Plan known as the Fast-Track Bike Plan that helped to identify and develop preliminary design concepts on projects that will be included in our "Phase 4". Along with those projects, we will be including a handful of smaller projects that will help to make practical and equitable connections to existing on-street routes and trails.

Describe existing conditions and need. Attach photos.

We are extremely proud of the progress we have made in Topeka over the past ten years to support bicycling as a key component of our transportation system. In 2010, the Shunga and Landon Trails stood out as jewels for recreational bicycling, but we lacked an on-street network and many other key ingredients to encourage bicycling for everyday transportation throughout the city. Since that time, our community has taken several important steps, including:

- Developed and adopted our first Bikeways Master Plan in 2012;
- Voted for a countywide half cent sales tax with up to \$3.5 million for bike projects through 2031;
- Developed Design Guidelines to strengthen the city and county Complete Streets policies;
- Built approximately 73 miles of on-street bike facilities including bike lanes (10%), multi-use side-paths (21%), and share the road signage or sharrows (69%);
- Received approximately \$2.4 million in KDOT grants for bicycling improvements;
- Launched the Topeka Metro Bikes bikeshare program;
- Received designation as a Bronze-Level Bicycle Friendly Community by the League of American Bicyclists;
- Adopted a new Land Use Growth Management Plan with Transportation choices as one of the major pillars; and
- Adopted the Futures 2040 Regional Transportation Plan that includes an increased emphasis on active transportation
- Developed the Topeka Fast-Track Bike Plan to serve as an Action Plan and Supplement to the 2012 Topeka Bikeways Master Plan. It prioritizes a network of **high-quality, low-stress** bike projects and renews our commitment to bicycling as a means to achieve the broader community goals of health, sustainability, equity, economic development, and high quality of life.

Significant progress has been made on making Topeka's Bikeways Master Plan a reality, but there is still a lot left to do. Topeka is at an important crossroads where it is critical to ensure that the next phase of bicycle infrastructure spending takes bicycling to a higher level in both quality and appeal to potential riders. The projects for this application were selected to respond to the evolving community priorities, while also utilizing current best practices. With our proposed plan we will be continuing to build upon the already great work that's been accomplished while adding significant connections that create additional access for the community.

The map on page 4 has been provided to show the existing bicycle infrastructure in Topeka.

We have grouped all of our projects within this application into 3 categories: downtown connections, trail connections and North Topeka connections. Below is the primary reasoning behind why we chose these projects and their impacts to Topeka.

Downtown Connections: There are several gaps in the bike network disconnecting downtown from surrounding neighborhoods and trails. Without these connections we cannot provide equitable and safe routes to downtown which is the regions major employment center. We will not only finish connecting several east-west streets (17th/15th, 12th, 8th, and 6th) but also will be closing the gap for the North-South routes connecting North Topeka to Downtown and Downtown to both the Landon and Shunga Trails. For most of the projects these will be new facilities that have been missing from the bike network. However, both the Kansas Avenue Bridge and 6th street will be upgraded from sharrows to separated facilities allowing the user to feel safer, comfortable and confident.

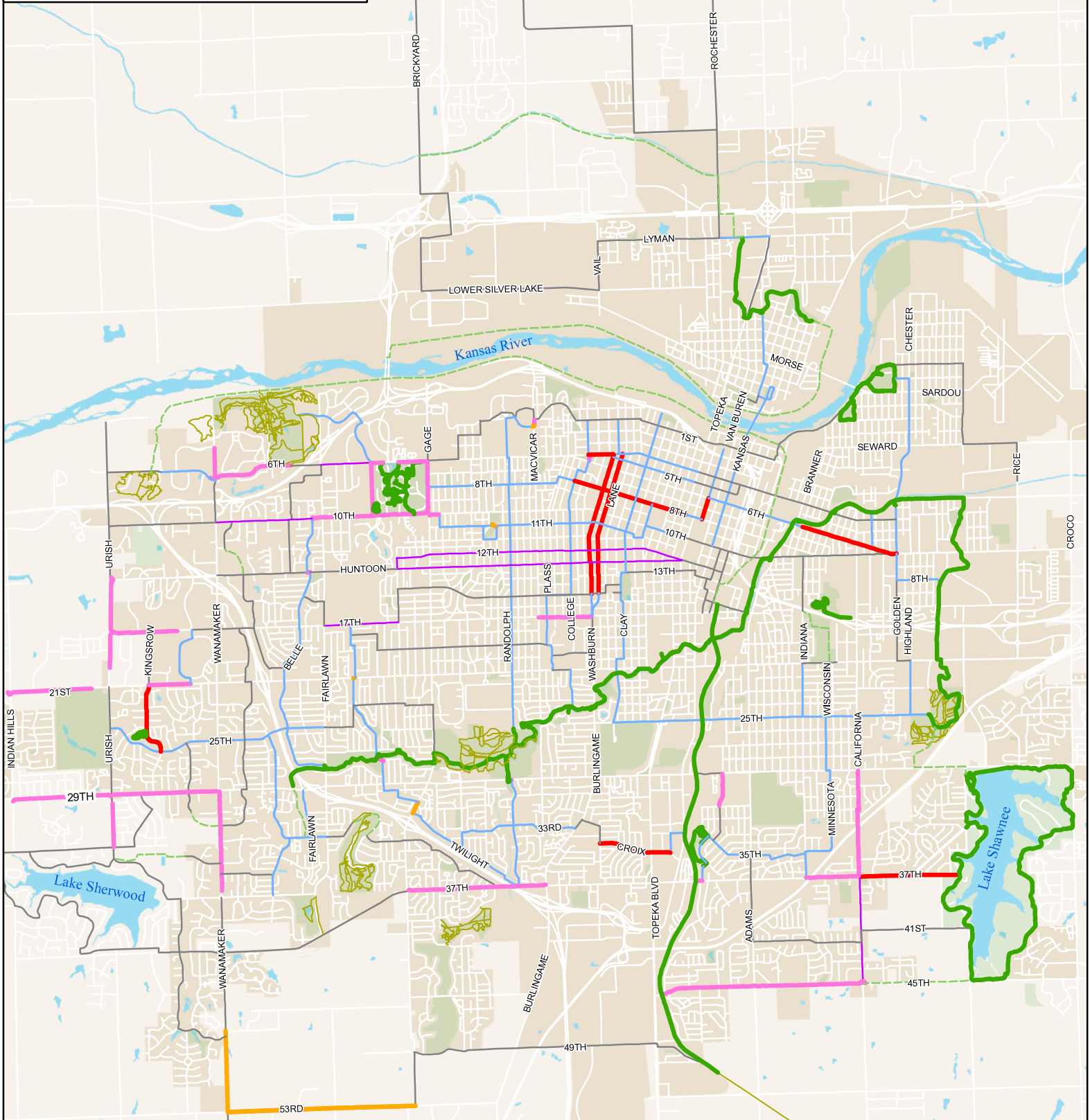
These projects take into account not only the Bikeways Master Plan and Fast-track update but also the current studies/plans and projects that are currently being conducted by the City of Topeka. The city is in the process of completing a Downtown Master Plan, working with consultants on the Polk-Quincy Viaduct road project and also has several street projects within and around the core area. We have met and consulted with each of the projects to ensure that our bikeways are cohesive as well as an integral part of their plans.

Trail Connections: The trail connections grouping was simply chosen for its ability to provide greater access to various locations along very popular hard-surfaced trails. These connections will provide access to a greater swath of neighborhoods that currently don't have easy access to the trails. Our intention's are to provide greater equity connections for the community, resulting in greater use of the trails and surrounding facilities.

North Topeka: Currently, North Topeka has the least amount of bicycle infrastructure within the area. There are several reasons why this may be the case but it usually comes down to outdated infrastructure, lack of space, or lack of complete streets elements being included in street projects. The city has been making great efforts to address the Complete Streets element with addition and implementation of the MTPO's Complete Streets Design Guidelines. Many street projects that would have advanced onto construction without even considering bike or ped related infrastructure, are now taking this into consideration during the design process. These projects are connected with a current City street reconstruction project that is being designed and now will include bike/ped amenities. The city plans on reconstructing Tyler from Lyman to Paramore by resurfacing, adding curb and gutter, adding sidewalks on both sides and adding a 10' sidepath trail connection from Waddell to the Soldier Creek Trail. The designs are currently being drawn up but there is still the chance that lack of funding may result in the sidepath being removed from the project.

Bikeways & Trails Facilities

As Funded through 2020

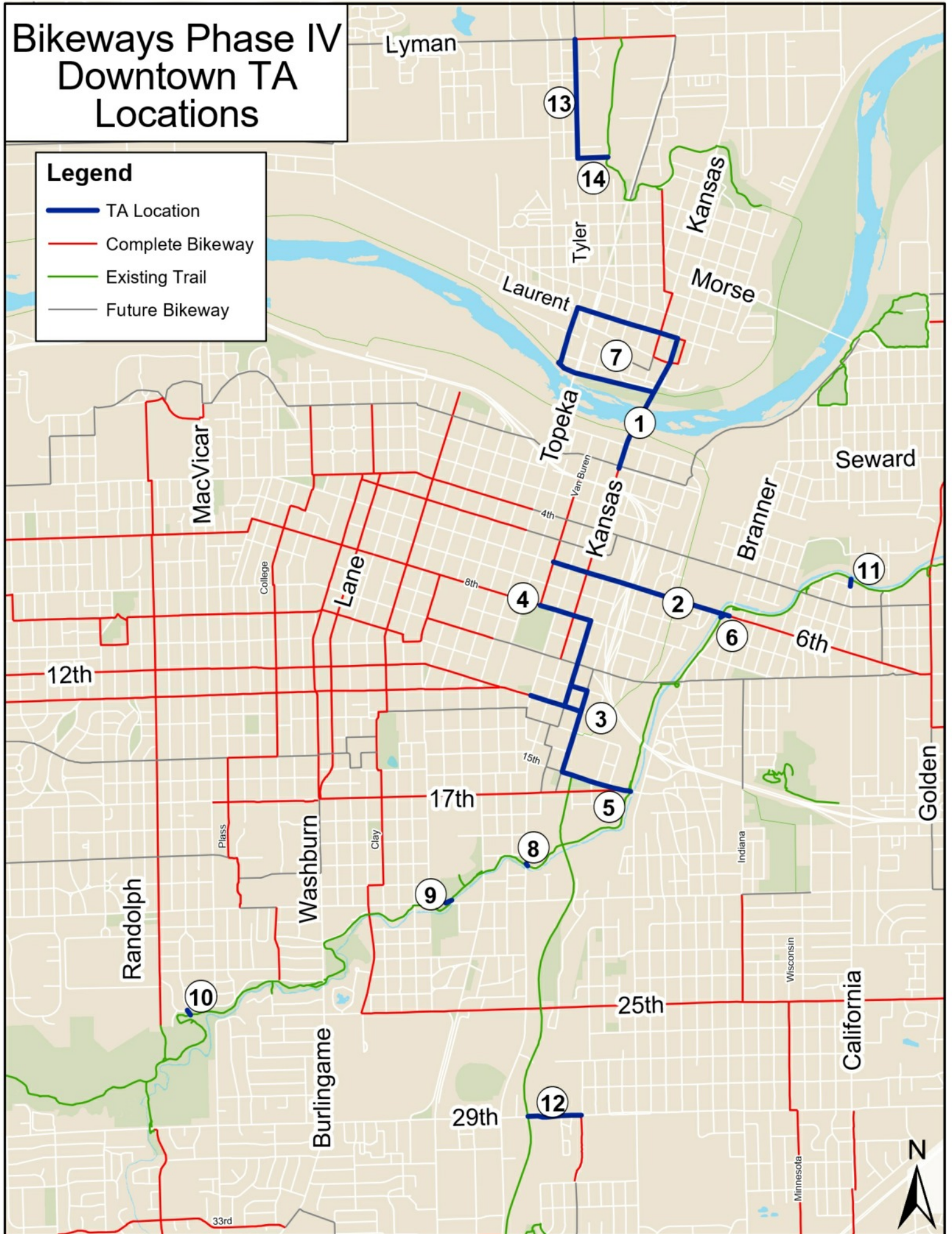


Legend	
—	Bike Lane - 7.41 Miles
—	Sidepath - 15.33 Miles
—	Sharrows - 46.28 Miles
—	Other - 4.47 Miles
—	Future Funded - 8.51 Miles
—	Future Not Funded - 79.85 Miles
—	Concrete Trail - 34.01 Miles
—	Nature Trail - 55.19 Miles
- - -	Future Trail - 22.66 Miles

Bikeways Phase IV Downtown TA Locations

Legend

- TA Location
- Complete Bikeway
- Existing Trail
- Future Bikeway



Describe the project proposal and detail scope. Include cost estimates for: total project cost; TA funds; and local share.

Phase 4 of the Topeka Bikeways Master Plan as of now consists of approximately 14 projects ranging in size and scope. We have grouped them into 3 categories: downtown connections, trail connections and North Topeka . Below is a list of the locations along with what the concept would potentially include:

Downtown Connections: \$1,408,160

1. Kansas Bridge – Laurent to 1st street: Separated or buffered bike lanes on the Kansas Avenue Bridge. **\$180,000**
2. 6th Avenue – Van Buren to Branner: Reduction of vehicle lanes and adding separated bicycle facilities and micro-surfacing **\$564,666.67**
3. Landon to downtown connection- Recommends the removal of one traffic lane on Monroe and the installation of a two-way separated bike lane that allows for northbound and southbound bike travel between the Landon trailhead at 15th and the 12th Street Bike Corridor. **\$288,066.67**
4. 8th Street from Topeka Blvd to Quincy: Sharrows and bike lanes and micro-surfacing **\$249,166.67**
5. 15th street from Landon Trailhead East to Shunga: Sharrows **\$2,000**
6. 6th and Branner connecting to the Shunga: 10' Trail connection **\$40,660**
7. North Levee Trail loop: Along the levee from Kansas to Topeka- Trail surface enhancements/signage/access. Tyler north to Laurent and Laurent to Kansas- Sharrows **\$83,600**

Trail Connections: \$122,564

8. 21st and Kansas connecting to the Shunga: 10' Trail connection **\$13,375**
9. 21st and Western connecting to the Shunga: 10' Trail connection **\$16,050**
10. Shunga connection somewhere between Morrow and MacVicar around 27th St: 10' Trail connection **\$13,910**
11. Liberty and California to the Shunga: 10' Trail Connection **\$10,379**
12. Landon Trail to Fremont on 29th: 10' Shared use path and retaining wall **\$68,850**

North Topeka: \$567,500

13. Tyler- Lyman to Waddell- Sidepath on the East side (design will be done with City Street recon project)
14. Waddell to Soldier Creek Trail: Trail Connection (design will be done with City Street recon project)

Attach additional pages that show schematic designs that illustrate basic project concepts.

The first three projects are from the Fast Track Bike Plan so we have included those pages that include high level drawings, designs and cost estimates. For the rest of the projects we have listed what type of infrastructure they would include, a cost estimate and attached pictures showing the approximate locations for improvements (shown in yellow).

The City has estimated it would take about **\$2,098,224** to complete all of the projects. We would be asking KDOT for **\$1,678,579.20** while the city would be contributing **\$419,644.80**.

We understand that there are limitations to the funding and this entire ask might not be possible. In the case that our application would need to be condensed, we have given each project its own cost estimate (on the following pages) as well as put them in a priority ranking that you see above to help with the selection.

1. Kansas Bridge from Laurent to 1st street - bike facilities, reduced vehicle lane

The Fast-Track Plan recommends separated or buffered bike lanes on the Kansas Avenue Bridge. The attached concepts provide several options for providing greater separation. The project would potentially include where possible: bike facilities, reduced vehicle lane, shortened ped crossing, and signal modifications.

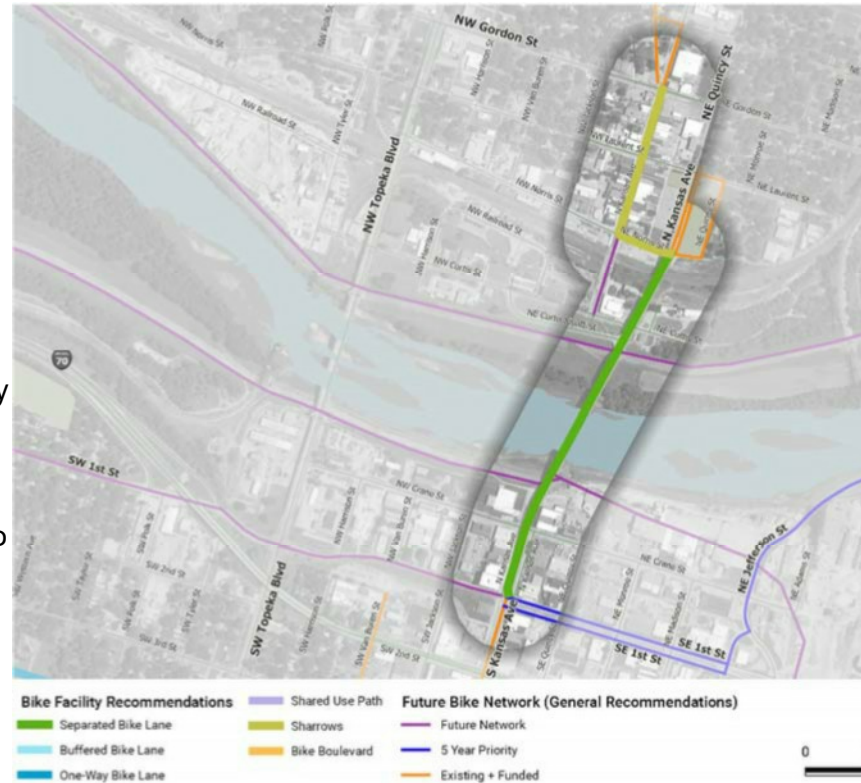
Cost estimate: \$180,000



Priority Corridor: Kansas Avenue Bridge, 1st Street to NOTO District (Portion of the North Topeka Bikeway-Route 23)

Overview

This segment connects W 1st Ave downtown to N Laurent Street and the Northern Topeka (NOTO) district and was included as part of the North Topeka Bikeway in the 2012 Plan. NOTO is a redeveloping commercial area that takes advantage of the area's attractive architecture and proximity to Topeka's downtown core. Bicyclists can use this corridor to traverse the river, which currently creates a pinch point for north- south travel. Additionally, this route allows bicyclists to connect to Soldier Creek Trail to the northwest. Finally, there are opportunities to make further connections south along a bikeway on Kansas Avenue though those details will be determined as part of the Polk-Quincy Viaduct project.



Existing Bicycle Facilities

The existing bridge configuration is four vehicular travel lanes (divided, with 2 lanes in each direction). There is shared sidewalk space on the west (southbound) side of the bridge protected by concrete barriers and shared lane (sharrow) markings in the outside travel lanes.

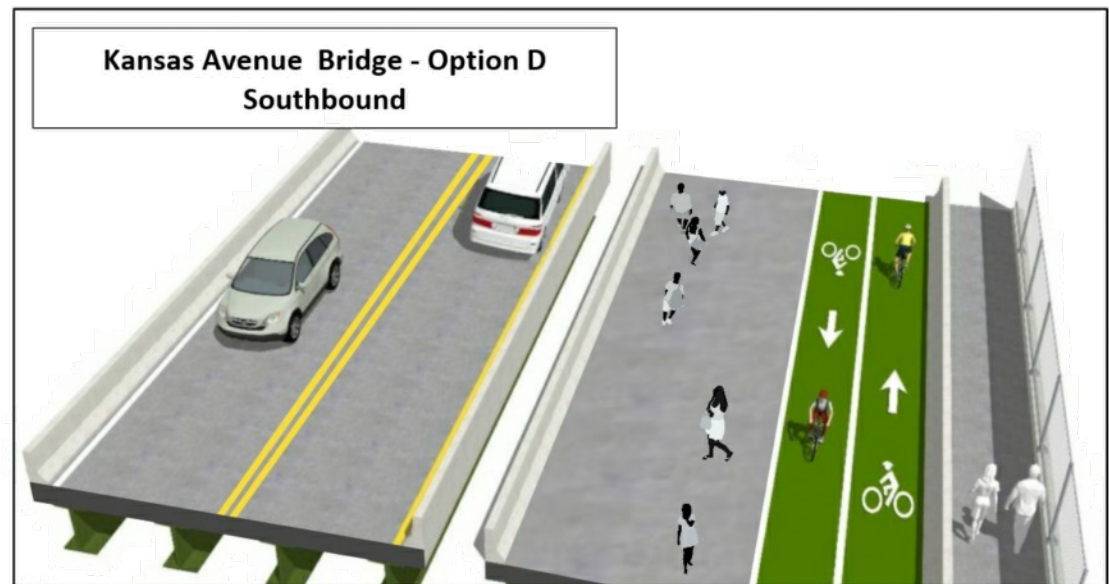
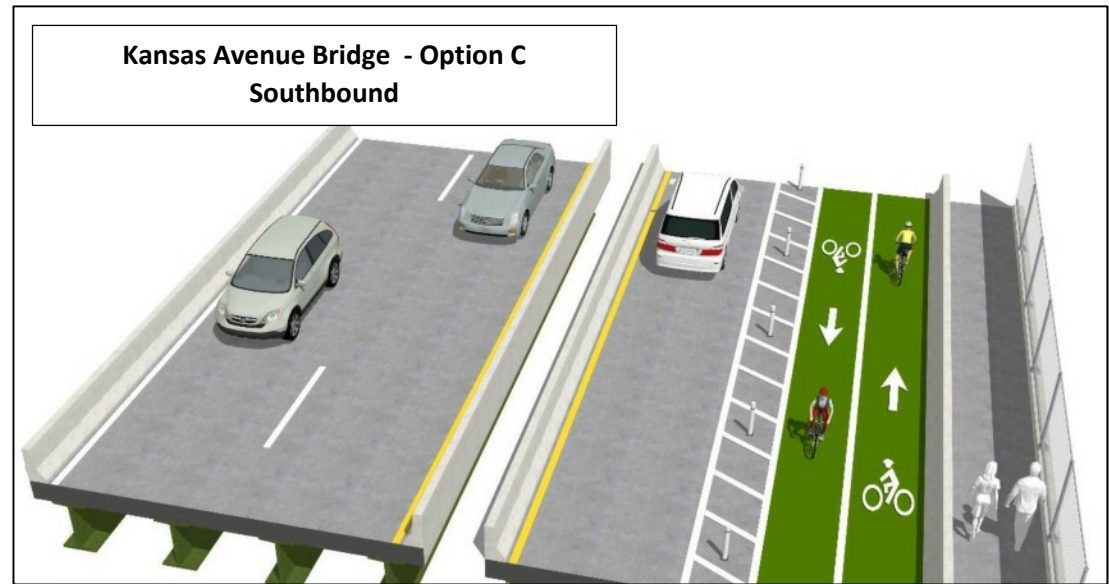
2012 Plan Recommendations

The 2012 Plan recommends sharrows in the short-term and the possible long-term implementation of colored advisory bike lanes.

Fast-Track Plan Recommended Bikeway Concept

This Fast-Track Plan recommends separated or buffered bike lanes on the Kansas Avenue Bridge. The following concepts were narrowed down from 4 concepts but will both provide options for providing greater separation. Options C would require only reducing travel lanes in the southbound direction. Options D would require eliminating vehicle traffic lanes on the east side of the bridge and having a vehicle travel lane going in both directions on the west side of the bridge.

These concepts are currently being evaluated by consultants to determine exact treatments and to get more accurate cost estimates. Both concepts shown here are at a planning level and will require further analysis as part of a full design process. **Cost Estimate:** Option C- \$38,000, Option D-\$TBD by consultants



Kansas Ave Bridge Transitions

North End

- A two-way cycle track, as reflected in Options B & C, allows for a more seamless transition on the North end of the bridge into NOTO. Sharrows on Laurent would guide bicyclists through NOTO and onward toward Central. Option A would require a more complex transition treatment on the north end of the bridge to cross several travel lanes of traffic from the east side of the bridge into NOTO.
- Additional design and analysis of turning movements will be required for this transition.

South End

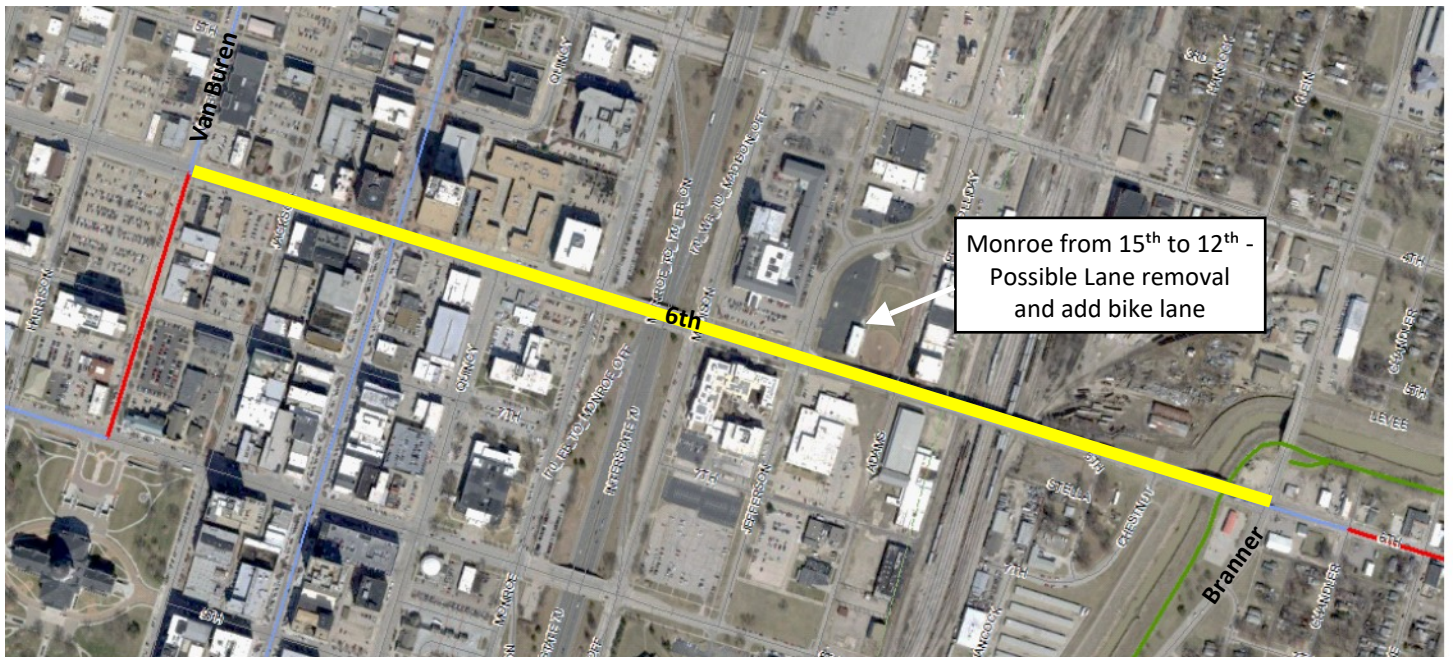
- For the south end transition, the space currently used for city and county parking shown in the screenshot below could be utilized for bicyclists to navigate under the bridge in order to access a two-way separated bike lane on the west side of the bridge. (Options B & C) Additional connections and bike facilities further South on Kansas will be examined as part of the Polk-Quincy Viaduct project.



2. 6th Avenue from Van Buren to Branner - bike facilities, reduced vehicle lane, shortened ped crossing, micro-surfacing and signal modifications.

The Fast-Track Plan recommends a buffered bike lane along this section of the East-West Bikeway Corridor. It is broken into two different segments based on the existing rights of way. The project would potentially include where possible: bike facilities, reduced vehicle lane, shortened ped crossing, micro-surfacing and signal modifications. This concept is currently being evaluated by consultants to determine exact treatments and to get more accurate cost estimates.

Cost estimate: \$564,666.67



Priority Corridor: SW 6th Avenue from SW Van Buren to SE Branner Trafficway (Portion of the East-West Bikeway-Route 1)

Overview

SW 6th Avenue between VanBuren and Branner Trafficway is a critical connecting portion of the East-West Bikeway identified in the 2012 Bikeways Master Plan. The route picks up where the existing bike lanes end at SE Branner Trafficway and the Shunga Trail and runs west to SW Van Buren where sharrows guide bicyclists south to the 8th Street bike facility. Bicyclists can use this corridor to traverse Interstate 70 and connect to the Shunga Trail from downtown. The route is currently characterized by wide streets (4 through lanes), and auto-oriented businesses. Some sections of the roadway have angled parking.

Existing Bicycle Facilities

Shared lane markings (sharrows)

2012 Plan Recommendations

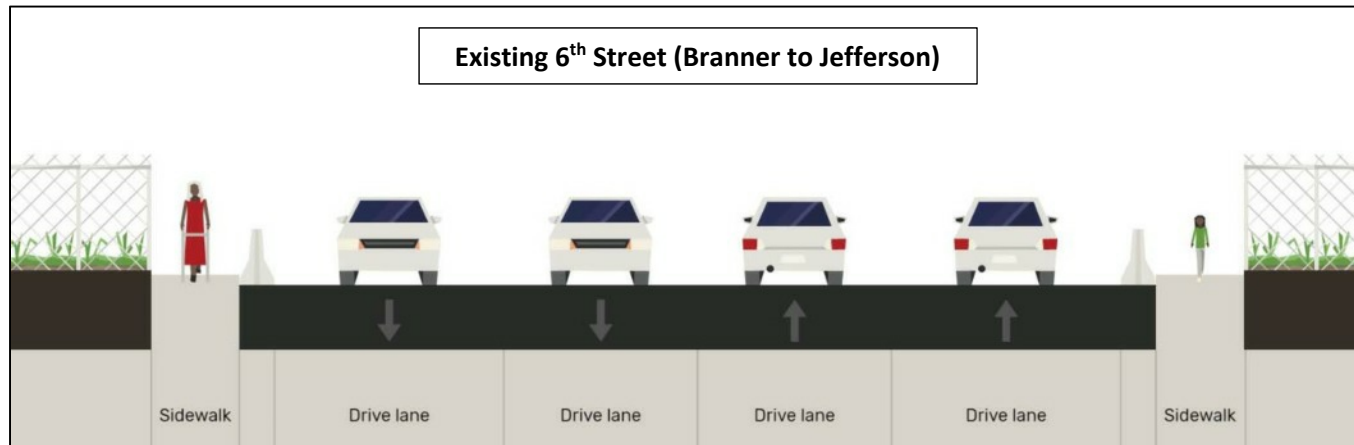
The 2012 recommends bike lanes and back-in angled parking along this route.

Fast-Track Plan Recommended Bikeway Concept

The Fast-Track Plan recommends a buffered bike lane along this section of the East-West Bikeway Corridor. It is broken into two different segments based on the existing rights of way. These concepts are at a planning level and will require further analysis as part of a full design process. **Cost Estimate: \$111,000**



From Branner Trafficway to Jefferson Street (Bridge Section)

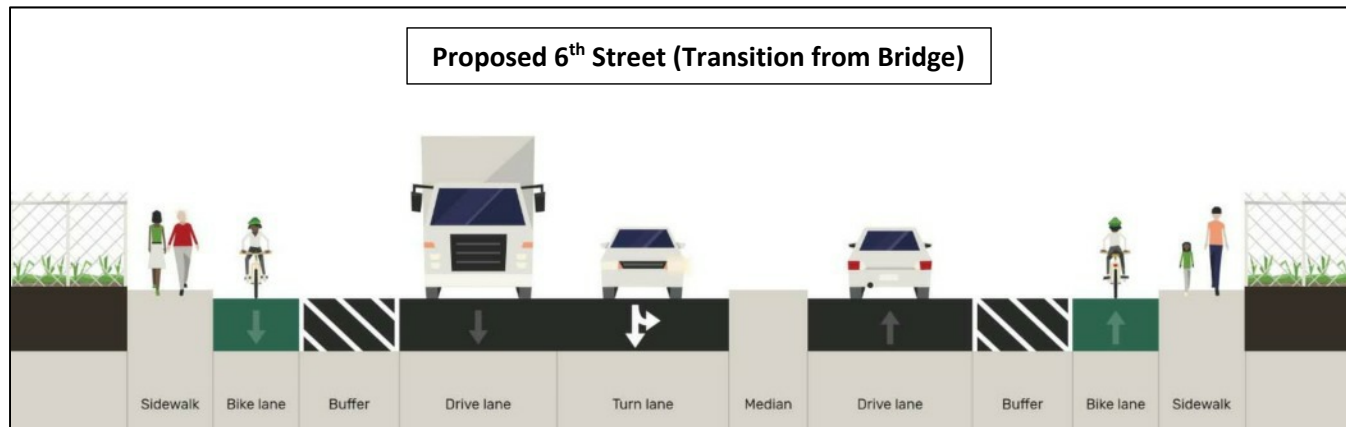
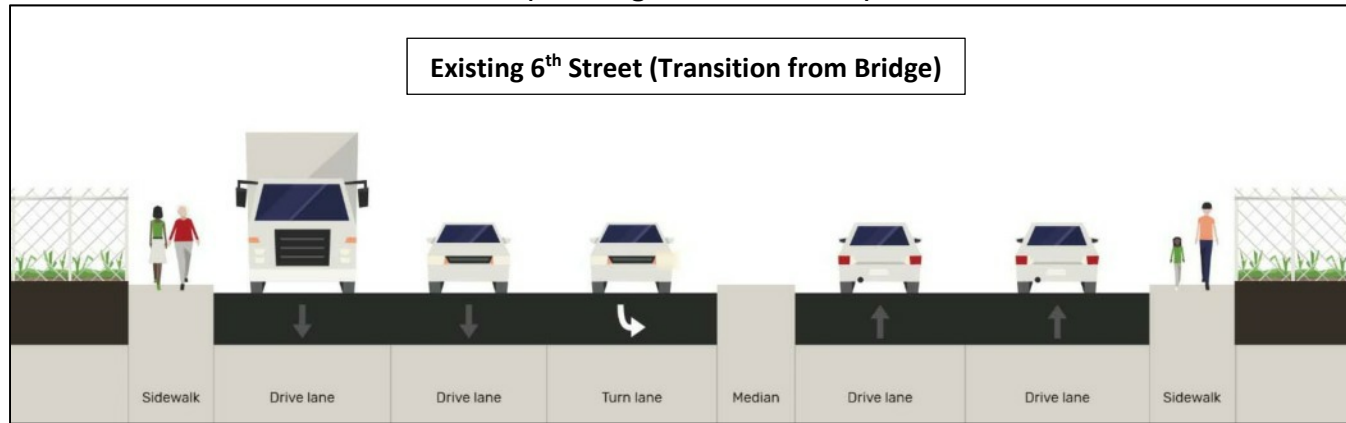


Recommendations

- Reduce exiting travel lanes from four to two to create space for one-way bike lanes on each side.

- Utilize wide painted buffer to create separation between motor vehicles and bicyclists. Provide vertical delineators approaching intersections to control the effective radius for turning vehicles and avoid motorist encroachment into bike lane approaching intersections.

Jefferson Street to Van Buren Street (Existing 5-Lane Section)



- Reduce five motor vehicle lanes to three to create space for buffered bike lanes on each side

- Utilize wide painted buffer to maintain continuity to bridge cross-section. Provide vertical delineators approaching intersections to control the effective radius for turning vehicles and avoid motorist encroachment into bike lane approaching intersections.
- Utilize back-in angled parking where parking exists to increase sight lines between motorists and bicyclists.



3. Landon to downtown connection- Where possible, we will be adding; bike facilities, reduced vehicle lane, added parking, minor intersection improvements, ADA modifications, shortened ped crossing, street beautification and improving of the ramp connection for the Landon Trailhead at 15th . The majority of this project was included in the Topeka Fast-Track plan. The high-level concepts and estimates our consultants provided for us are attached. The city has also been doing several street projects around this area. They have resurfaced Monroe and will be reconstructing Quincy from 8th -10th streets. Unfortunately, due to time constraints we were unable to get any bike facilities down on Monroe. However, we were able to have some preliminary designs drawn up for Monroe 15th-12th streets and from Monroe to Kansas Ave. These drawings are attached. As for the Quincy segment, this would be the perfect opportunity to collaborate with this street project, adding a buffered bike lane offering a safer route than using the busy arterial, Kansas Ave. This project will provide a huge missing north-south link that provides connections and access from the Landon Trail all the way up to downtown as well as tying in the east-west connections along 15th/17th, 12th, and 8th streets. This project will also connect with projects #4 and #5.

Cost estimate: \$288,066.67



Priority Corridor: Landon Trail Connector via Monroe, 15th Street to 12th Street (Portion of the 11th Street Bikeway-Route 22)

Overview

This route connects the popular Landon Nature Trailhead with Topeka’s central business district along Monroe, passing the Brown v. Board of Education Historic Site and connecting to the 12th Street Bikeway. This improvement would extend the reach of Landon Nature Trail, bringing it into the heart of Topeka. The corridor is characterized by industrial and institutional land uses in the southern section, and commercial land uses in Topeka’s central business district.

Existing Bicycle Facilities

None

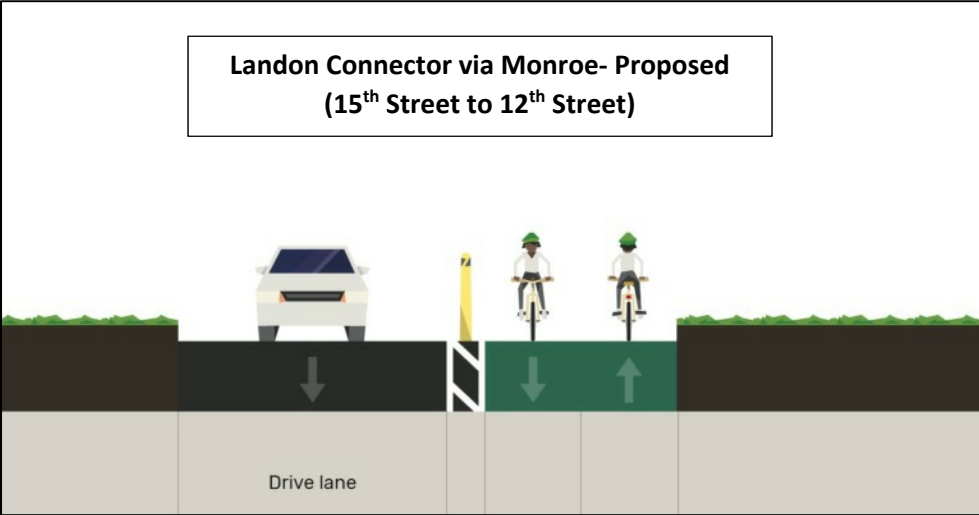
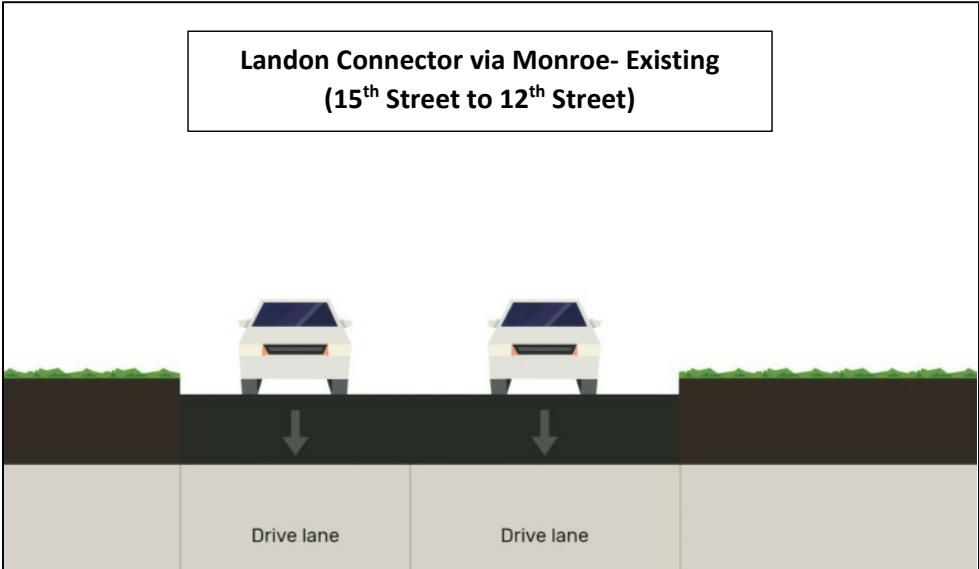
2012 Plan Recommendations

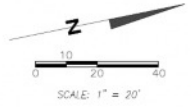
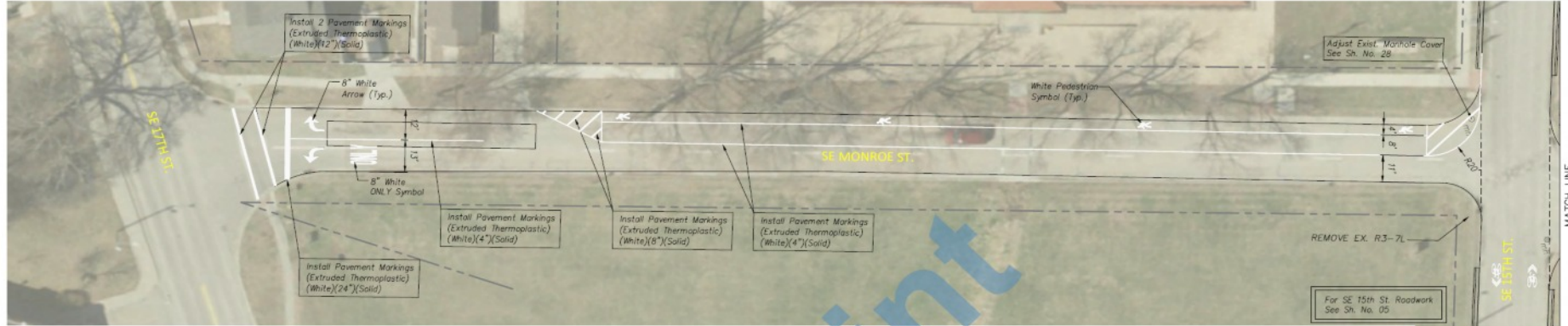
The 2012 Plan recommends a pair of bike lanes on Monroe and Quincy.

Fast-Track Plan Recommended Bikeway Concept

This plan recommends the removal of one traffic lane on Monroe and the installation of a two-way separated bike lane that allows for northbound and southbound bike travel between the Landon trailhead at 15th and the 12th Street Bike Corridor. These concepts are at a planning level and will require further analysis as part of a full design process. **Cost Estimate:** \$234,000







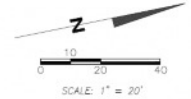
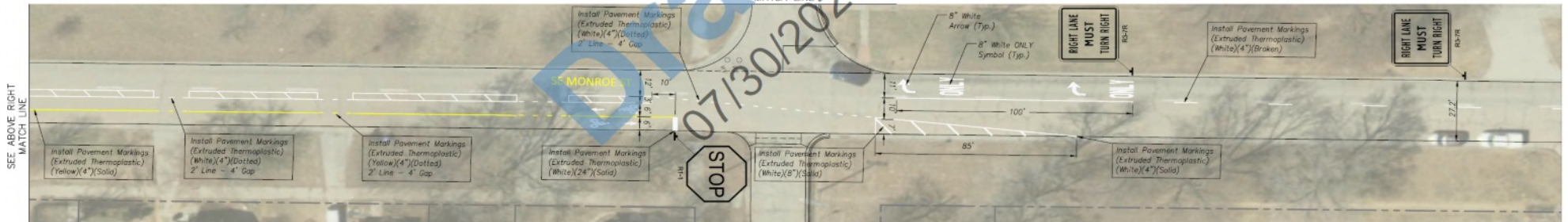
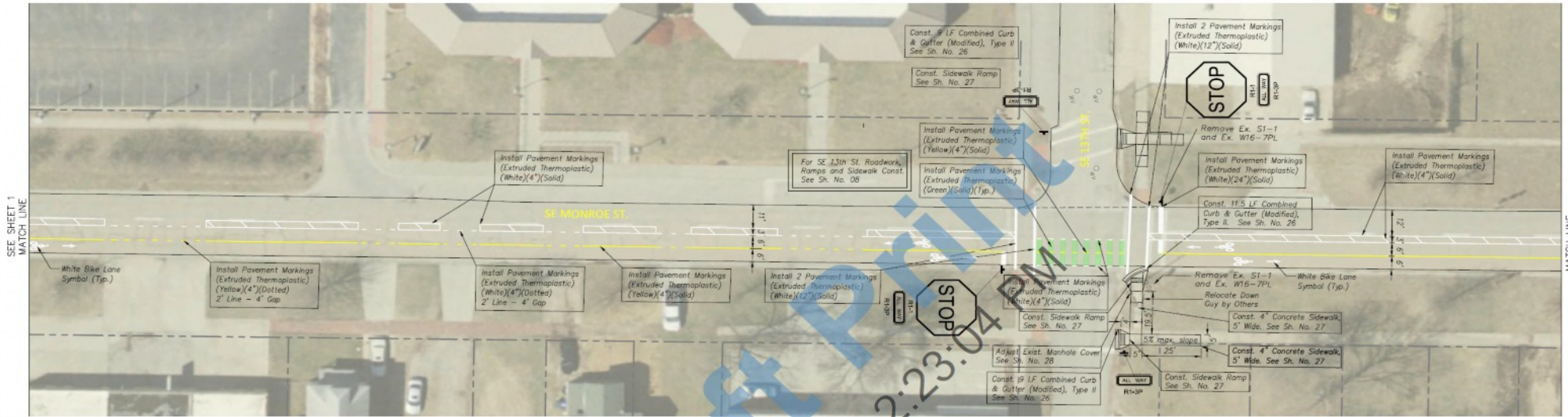
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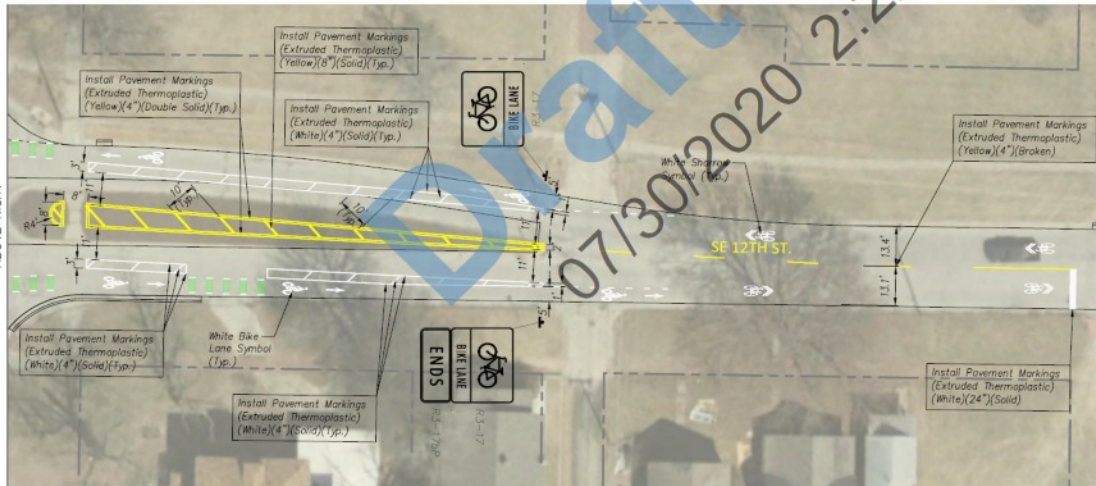
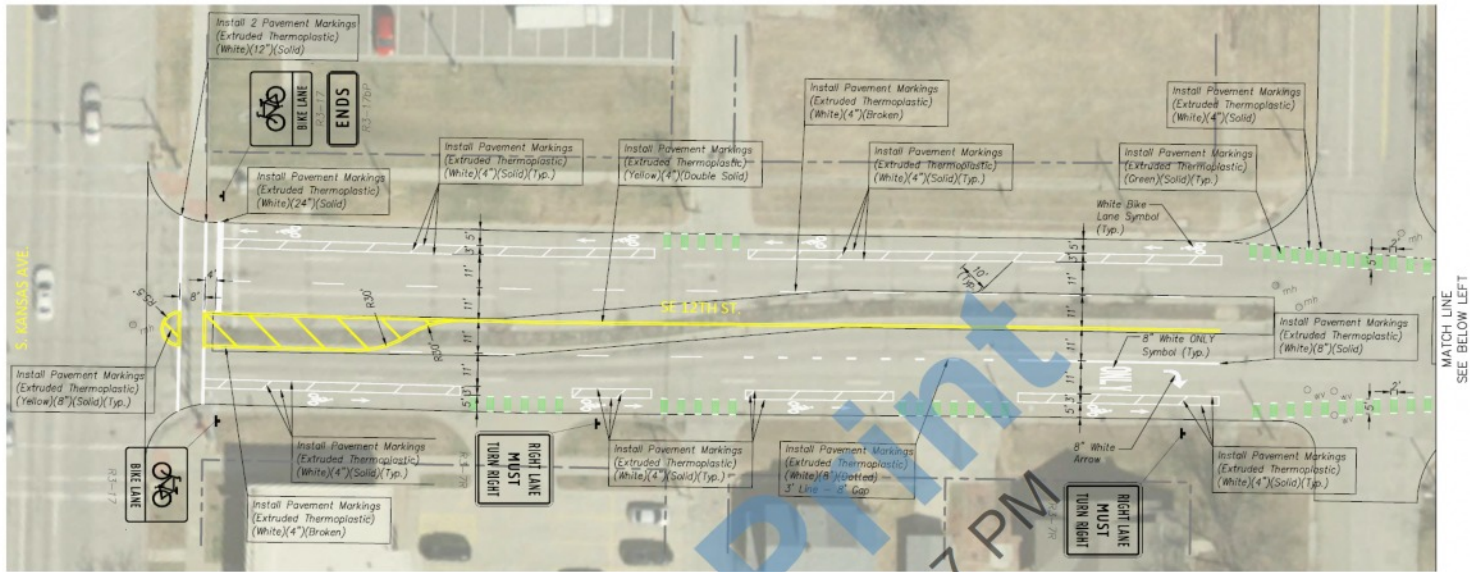
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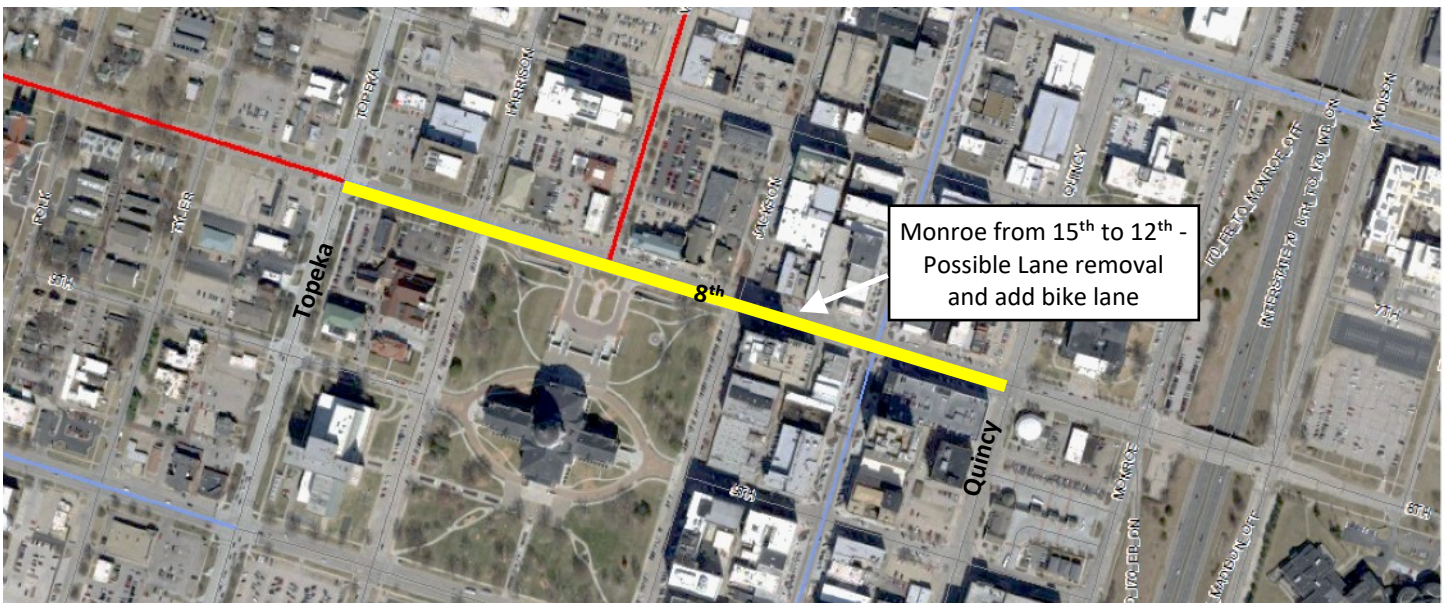




4. 8th Avenue from Topeka to Madison – Sharrows or bike lanes, micro-surfacing, shortened ped crossings and signal modifications

Sharrows and bike lanes. The project would potentially include where possible: bike facilities, reduced vehicle lane, shortened ped crossing, micro-surfacing and signal modifications. This concept is currently being evaluated by consultants to determine exact treatments and to get more accurate cost estimates.

Cost estimate: \$249,166.67



5. 15th from Landon Trail exit to Shunga Trail – Sharrows

These sharrows are a part of the original Bikeways Master Plan and will connect both the Landon to the Shunga Trail and also connect to project #3.

Cost estimate: \$2,000



6. 6th @ Branner to Shunga- 10' Trail Sidepath

This particular trail connection came to our attention via our Complete Streets Advisory Committee. It was brought to our attention that there was no easy way to access the Shunga Trail south of 6th street. Adding a south access point would provide a safer and more appealing option for the community. It would provide direct access to both the downtown neighborhood and the East Topeka South Neighborhood.

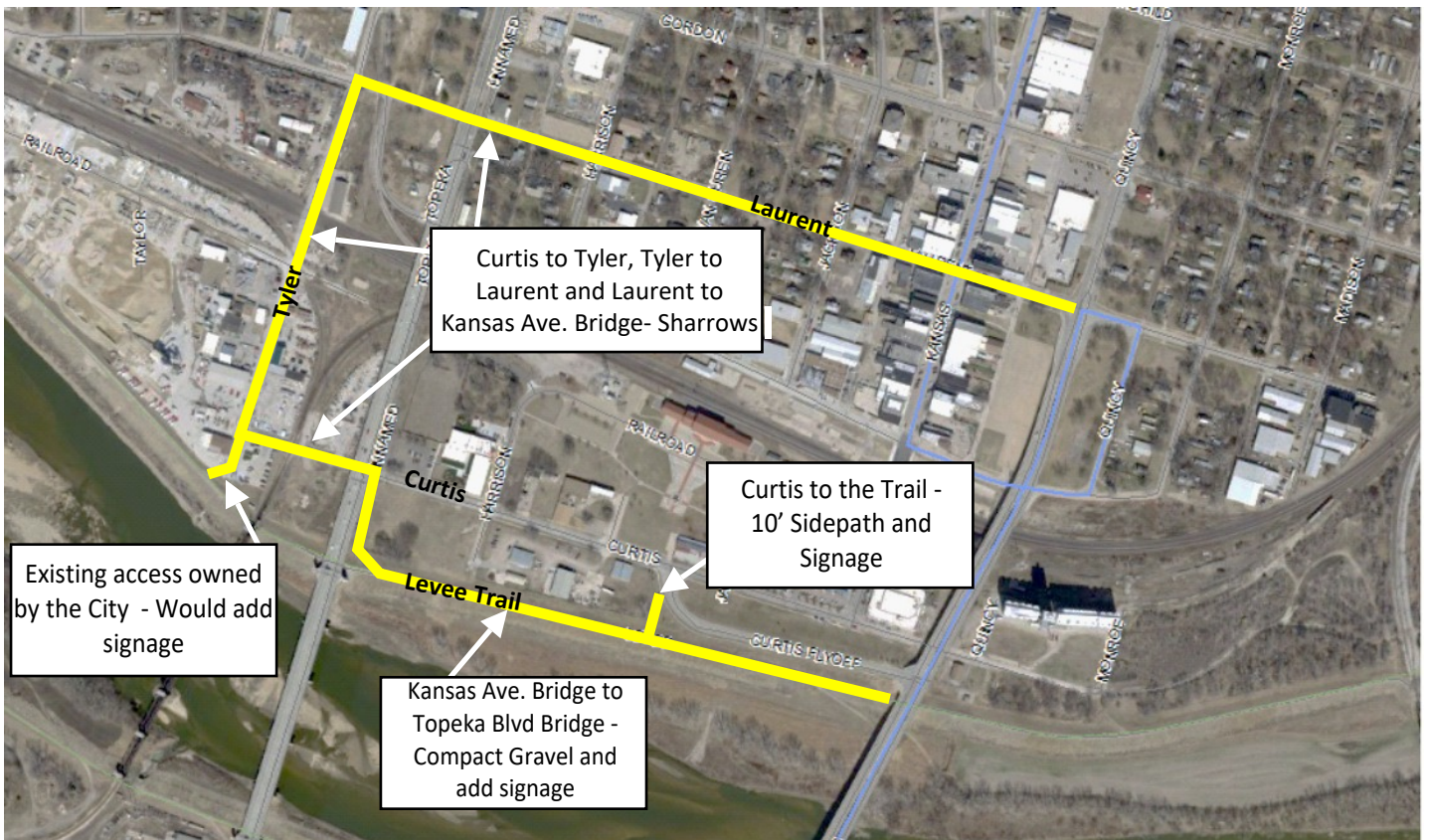
Cost estimate: \$40,660



7. North Levee Trail loop- Signage and trail enhancements on levee, sharrows on Tyler and Laurent, and a 10' sidepath from Curtis to the Trail

This project would take utilize the existing North Levee Trail and create additional access points as well as a connecting loop through North Topeka (NOTO). We would compact the existing gravel between the Topeka and Kansas Avenue Bridge to make it a little easier to handle for users, an access point created to connect the trail to Curtis Street with a 10' sidepath and signage added. Currently, there is a path that takes you from the trail up to Curtis Street and from there we would add sharrows east to Tyler, North to Laurent and East to the Kansas Avenue Bridge, effectively creating a loop. This route bypasses the railroad crossing to the east of Topeka Blvd, providing a safer route for the users. This bypass will take you to the cross streets of Tyler and Curtis. Just south of this intersection, there is an access point that takes you through an adjacent property. This access point is an existing land/ROW and a trail easement that the city currently owns. Signage would be added to help guide users through this area.

Cost estimate: \$83,600



8. 21st @ Kansas to Shunga – 10' Trail Sidepath

This trail connection came to our attention via our Complete Streets Advisory Committee. It was brought to our attention that there was no easy way to access the Shunga Trail on the east side of Kansas Avenue. Without this connection a user must exit the trail further east and then cross Kansas Avenue at an unmarked crossing. Adding an east access point would provide a safer and more appealing option for the user. The Landon Trail is also in close proximity to the east of Kansas Ave and would benefit from the additional access point. This connection would be beneficial to the entire community but would directly affect the Monroe Neighborhood.

Cost estimate: \$16,050



9. 21st @ Western to Shunga –10' Trail Sidepath

This particular trail connection also came to our attention via our Complete Streets Advisory Committee. It was noted that there was no easy way to access the Shunga Trail north of 21st street. Without this connection a user can only exit the trail on the south side of 21st where there are no designated crossings. Adding a north access point would provide a safer and more appealing option for the user. This connection would affect the entire community but would have a direct benefit to the Chesney Park Neighborhood.

Cost estimate: \$13,375



10. Shunga to somewhere around Murrow or MacVicar – 10' Trail Sidepath

This particular trail connection also came to our attention via our Complete Streets Advisory Committee. It was noted that there was no easy way to access the Shunga Trail north off of Shunga Drive. Without this connection a user can only exit the trail much further north or south making this neighborhood hard to access. Adding a north or east access point would provide a more inclusive and appealing option for the all users and would be directly beneficial to the Central Topeka Neighborhood. At this location, we do have several options for the location for the trail. Depending on circumstances we could put a sidepath that connects to either Shunga Drive or on Murrow. For our purposes I have included a cost estimate for the larger of the two connections.

Cost estimate: \$13,910



11. Liberty and California to the Shunga: 10' Trail Connection

This particular trail connection also came to our during the City's SORT (Stages of Resource Targeting). The city is currently doing work within the East Topeka North Neighborhood that would include improving the public infrastructure. Sidewalks are a key part of this program and by providing more opportunities for residents to be walking in their neighborhoods we would also like to help provide access to the Shunga. Without this connection a users only options are accessing the trail much further east or west making this neighborhood hard to access as well as.

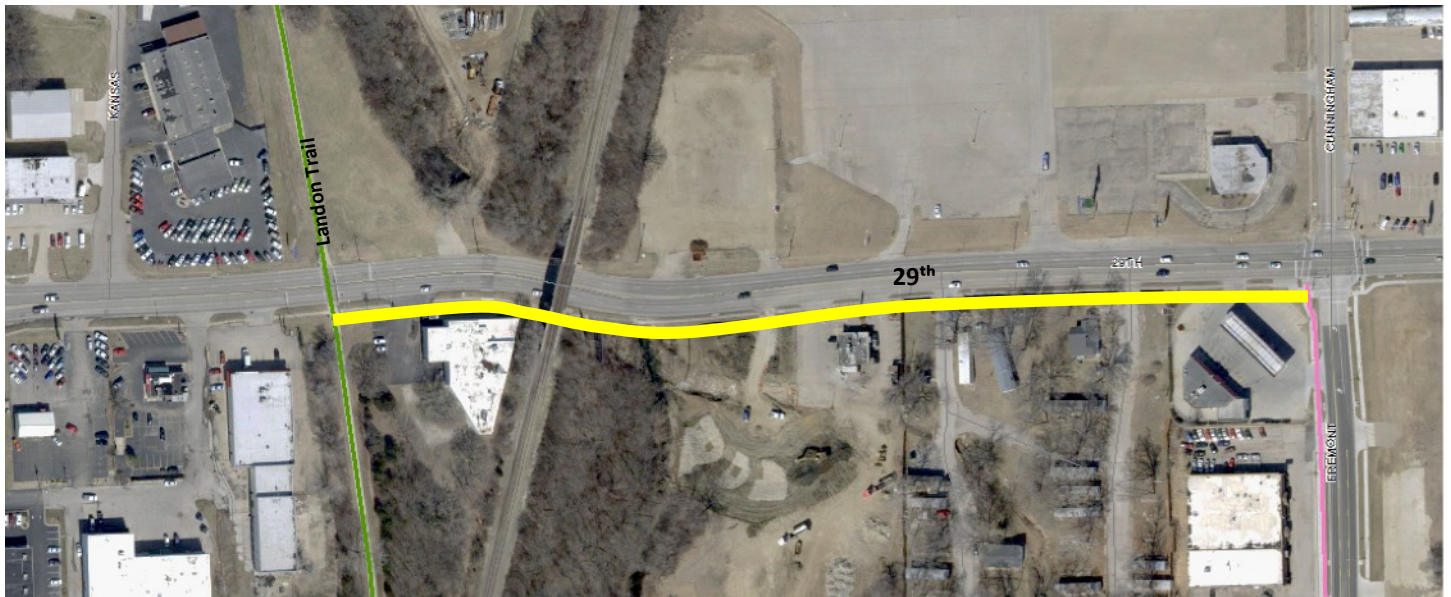
Cost estimate: \$10,379



12. Landon Trail to Fremont on 29th - 10' Sidepath and retaining wall

This trail connection was also given to us via our Complete Streets Advisory Committee. This project would take the existing sidewalk on south side that is currently very narrow and uninviting and replace it with a 10' sidepath. This sidepath addition would require a retaining wall underneath the bridge. This link would directly connect the Landon Trail to the Hi-Crest Neighborhood providing a safer and more inclusive area for both bike and peds.

Cost estimate: \$59,850



13/14. Lyman to Waddell on Tyler and connecting to Soldier Trail – Sidewalks on both sides and sidepath to the trail.

There has been a consistent need and desire for sidewalks within North Topeka. Many streets within the area lack curb and gutter making it more difficult and more expensive to provide sidewalks. However, the city plans on reconstructing Tyler from Lyman to Paramore by resurfacing, adding curb and gutter, adding sidewalks on both sides and adding a 10' sidepath trail connection from Waddell to the Soldier Creek Trail. This provides the perfect opportunity to include a non-vehicular amenity and would be accommodating the residence of North Topeka West Neighborhood. Many residence have stated that they do currently utilize Tyler to get north to their local grocery stores and feel that it is currently unsafe in its current condition.

The designs are currently being drawn up but there is still the chance that lack of funding may result in the sidewalks being removed from the project.

Cost estimate: \$567,500

Sidewalks - \$492,600

Sidepath - \$74,900

